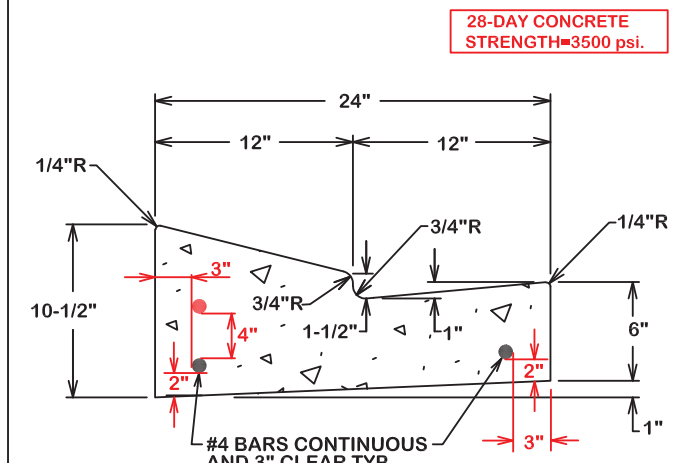


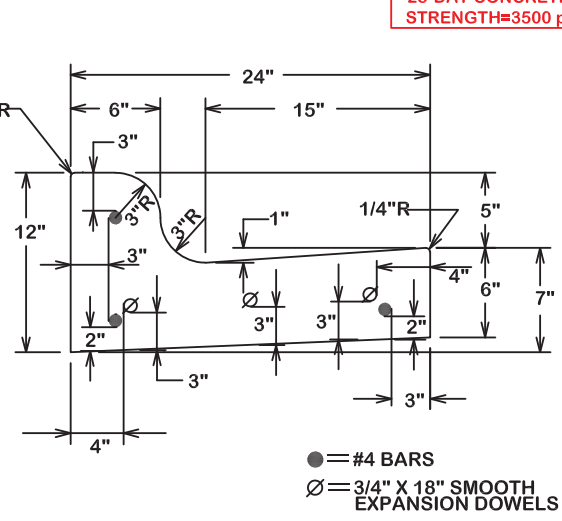
NOTE:
TYPE "G" EXPANSION JOINTS IN CURB & GUTTER SHALL BE SPACED AT A MAXIMUM DISTANCE OF 40' APART AND AT ALL RADIUS POINTS, P.T.'S AND P.C.'S. TYPE "B" CONTRACTION JOINTS IN CURB & GUTTER SHALL BE SPACED AT A MAXIMUM DISTANCE OF 10' APART.



24" LAY DOWN
GUTTER SECTION

ST1-00

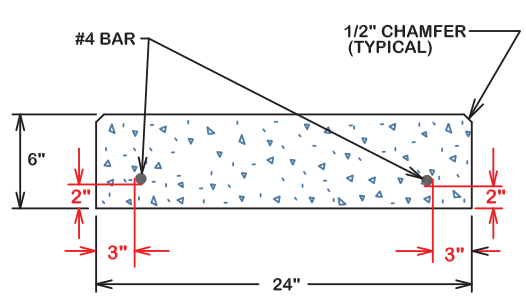
NOTE:
TYPE "G" EXPANSION JOINTS IN CURB & GUTTER SHALL BE SPACED AT A MAXIMUM DISTANCE OF 40' APART AND AT ALL RADIUS POINTS, P.T.'S AND P.C.'S. TYPE "B" CONTRACTION JOINTS IN CURB & GUTTER SHALL BE SPACED AT A MAXIMUM DISTANCE OF 10' APART.



TYPICAL COMBINED CURB & GUTTER SECTION

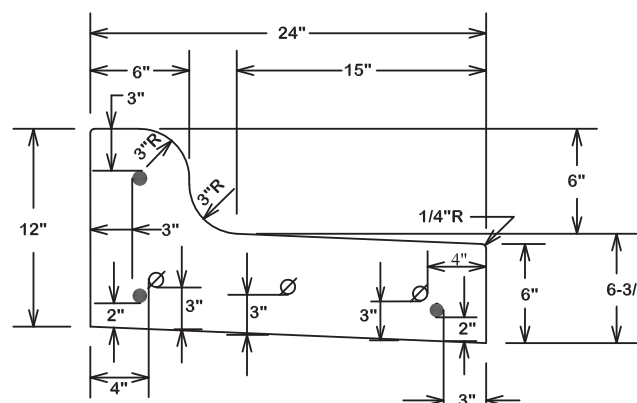
ST1-01

NOTE:
TYPE "C" EXPANSION JOINTS IN CURB & GUTTER SHALL BE SPACED AT A MAXIMUM DISTANCE OF 40' APART AND AT ALL RADIUS POINTS, P.T.'s, AND P.C.'s. TYPE "B" CONTRACTION JOINTS IN CURB & GUTTER SHALL BE SPACED AT A MAXIMUM DISTANCE OF 10' APART



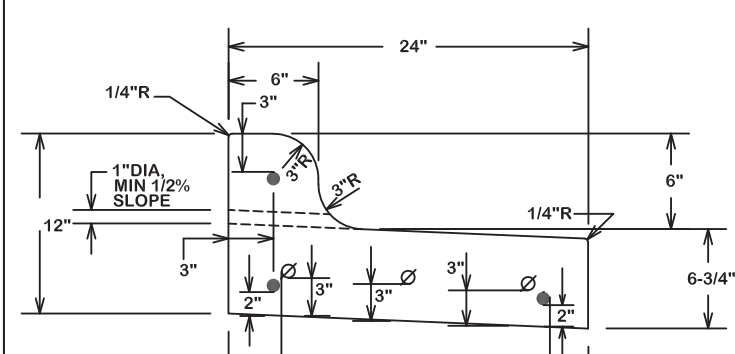
TYPICAL RIBBON CURB

ST1-03



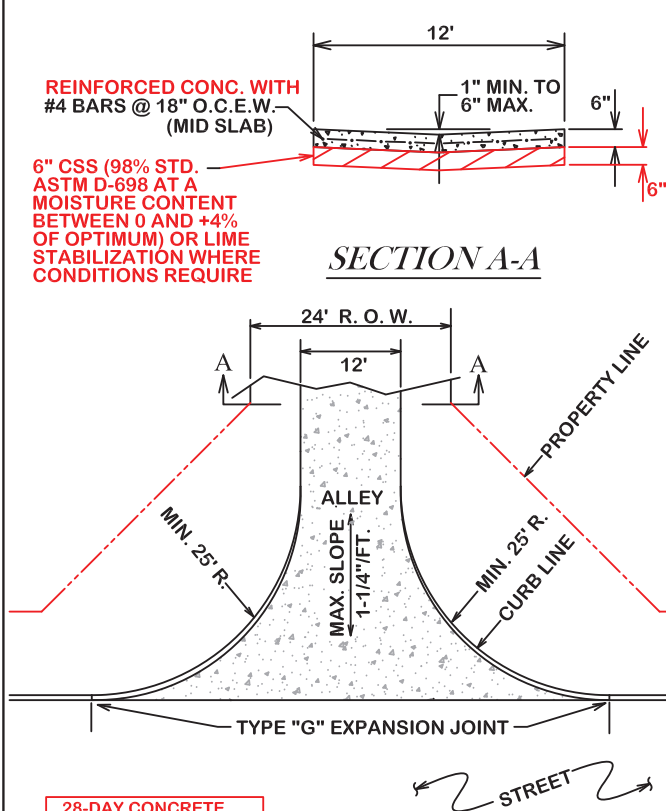
TYPICAL COMBINED SPILL CURB

ST1-04



TYPICAL WEEPHOLE CURB

ST1-05



ALLEY PAVING

ST1-06

GENERAL NOTES:

APPROVED EROSION CONTROL MEASURES MUST BE INSTALLED DURING THE ENTIRE TIME THAT EARTH HAS BEEN BARED BY CONSTRUCTION AND SHALL STAY IN PLACE UNTIL ACCEPTABLE VEGETATIVE GROWTH IS ESTABLISHED AFTER CONSTRUCTION IS COMPLETE AND THEN REMOVED BY CONTRACTOR.

ALL EROSION CONTROL MEASURES SHOULD BE CLEANED OF SILT AFTER EVERY RAIN.

ALL TRAFFIC SIGNALS AND APPURTENANCES, AND ALL PAVEMENT MARKINGS AND MARKERS SHALL BE IN ACCORDANCE WITH TxDOT STANDARDS

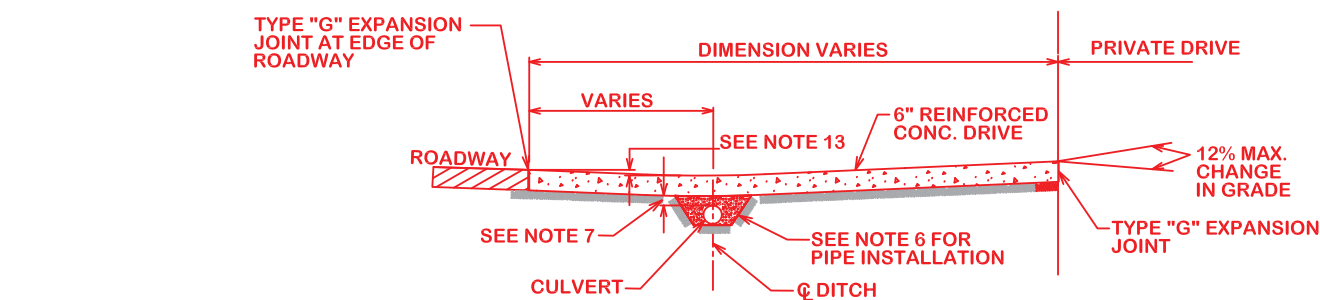
REFER TO SPEC 31 17 23.23 (PAVEMENT MARKINGS) FOR ADDITIONAL LOCAL REQUIREMENTS.

RESIDENTIAL DRIVEWAY

ST2-00

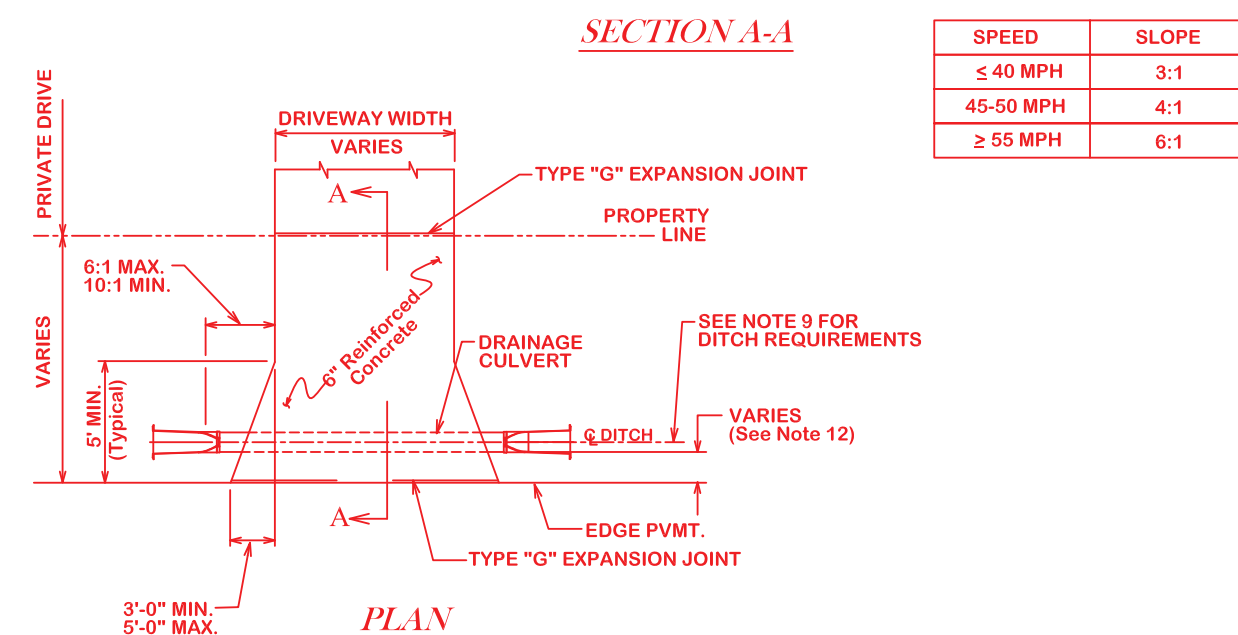
NOTE:

- | | | |
|---|---|---|
| 1. LENGTH OF DRAINAGE PIPE TO RESIDENTIAL DRIVE IS 30' MAX & 20' MIN. | 9. DITCH NEEDS TO BE CLEANED BOTH ENDS TO MATCH DITCH SLOPE. | 13. DRIVEWAY ELEVATION OVER THE CURB MUST BE A MINIMUM OF 12" AND SURFACE WHEN DIRT AND MAXIMUM OF 12" ABOVE ROAD SURFACE IF A HUMPS. |
| 2. ALL DRAIN PIPES WITHIN ROW MUST HAVE CONCRETE 2:1 (SLOPE) END APPROVED ELEVATION. | 10. SLOPES TRANSVERSE TO THE PUBLIC STREET (i.e., DRIVEWAY SIDE SLOPES) MUST BE FLAT AS PRACTICAL WHERE PRACTICAL. A SLOPE OF 1:1 (SLOPE) IS ALLOWED (1:1.6H) OR FLATTER SHALL BE CONSTRUCTED WITH A MINIMUM SLOPE OF 1:1.6H BEING ALLOWED. IN NO INSTANCE SHALL THE SLOPE BECEDED VARY WITHOUT SPECIFIC CITY APPROVAL. | |
| 3. CULVERT DIAMETER TO BE APPROVED BY THE CITY ENGINEER. THE SIZE OF THE DRIVE CULVERT SHALL BE A MINIMUM OF D12" AND MAN DESIGNER FOR A LEAST A 10-YR STORM EVENT. THE SLOPE OF THE PIPE SHALL BE A MINIMUM OF 0.5%. | 11. DRIVEWAY CULVERTS: | |
| 4. CULVERT SHALL BE RCP OR HDPE. | A. SHALL BE PLACED AS FAR AWAY FROM THE PUBLIC ROADWAY AS PRACTICAL. | |
| 5. MEETING THE CITY'S SPECIFICATIONS FOR STORM CULVERT IS NOT ALLOWED. | B. SHALL BE CUT TO MATCH THE DRIVEWAY SLOPE. | |
| 6. BELL AND SPIGOT GASKETED JOINTS OR MANUFACTURED COUPLING BANDS ARE REQUIRED WITH HOPE. | C. SHALL HAVE DELINEATORS PLACED AT EACH END FOR HAZARD IDENTIFICATION. | |
| 7. CEMENT STABILIZED SAND IS REQUIRED WITH HOPE. | | |
| 7. THERE SHALL BE A MINIMUM OF 12" COVER FOR HDPE PIPE AND 6" COVER FOR RCP PIPE. | 12. REFER TO THE CURRENT ADDITION OF THE ASHITO ROADSIDE DESIGN GUIDE FOR ADDITIONAL INFORMATION ON THE CONSTRUCTION OF THE DRIVEWAY AND THE PLACEMENT OF THE DRIVEWAY. | |
| 8. TOP OF CONCRETE DRIVE TO BE LEVEL WITH ROAD SURFACE. | | |



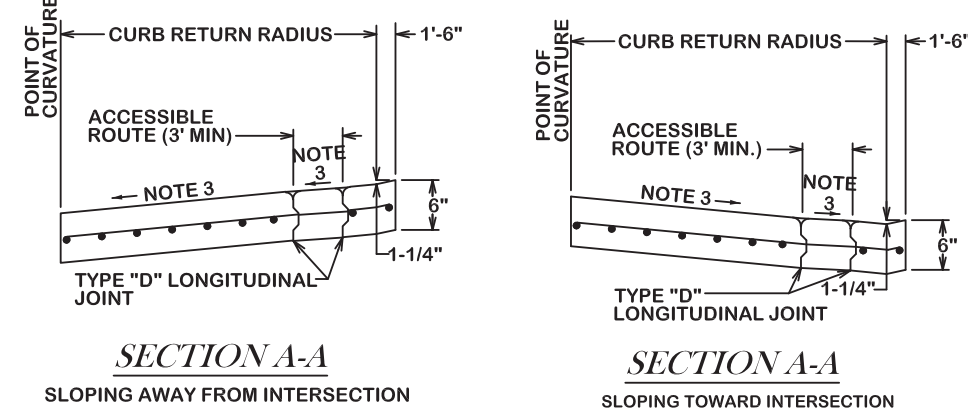
TYPICAL DRIVEWAY ENTRANCE WITH CULVERT

ST2-02



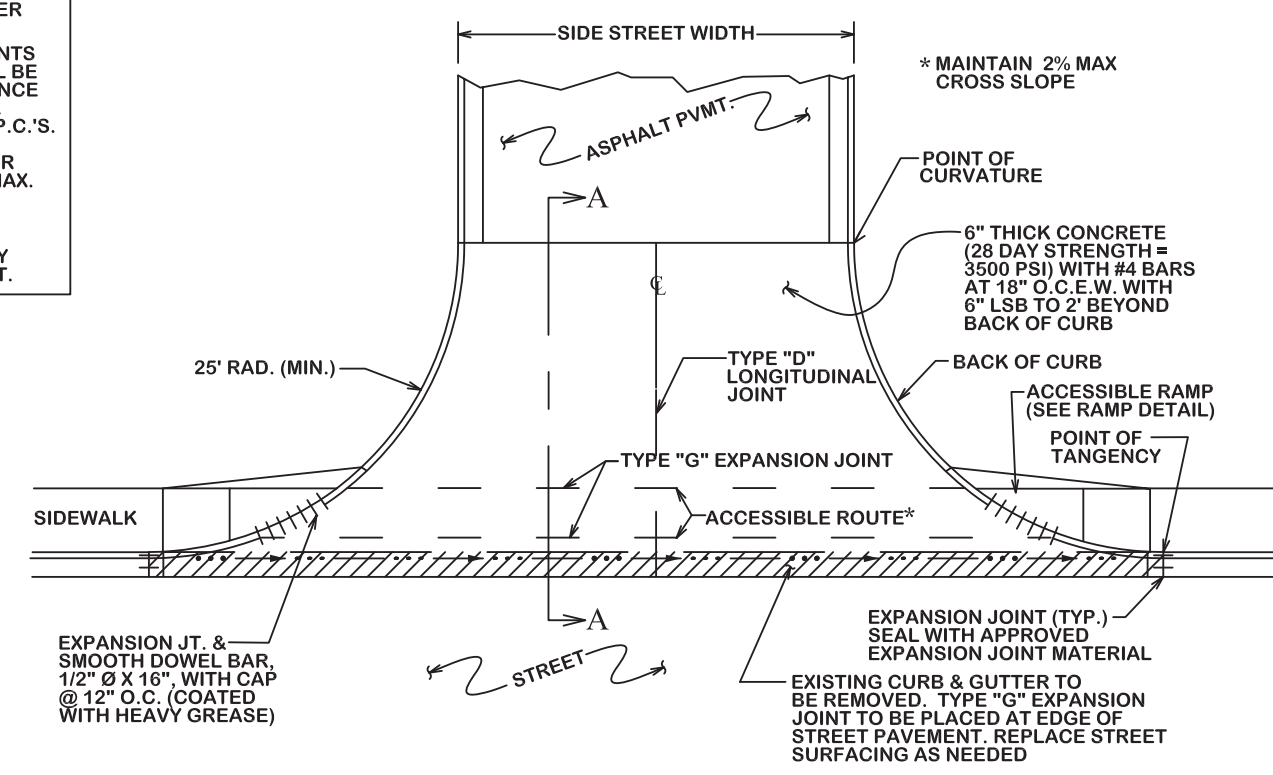
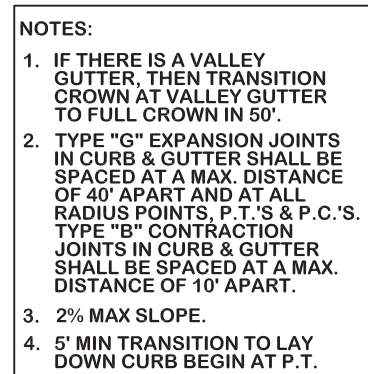
COMMERCIAL DRIVEWAY

ST2-03



TYPICAL STREET CONCRETE APRON

ST2-01



EXISTING CURB & GUTTER TO BE REMOVED

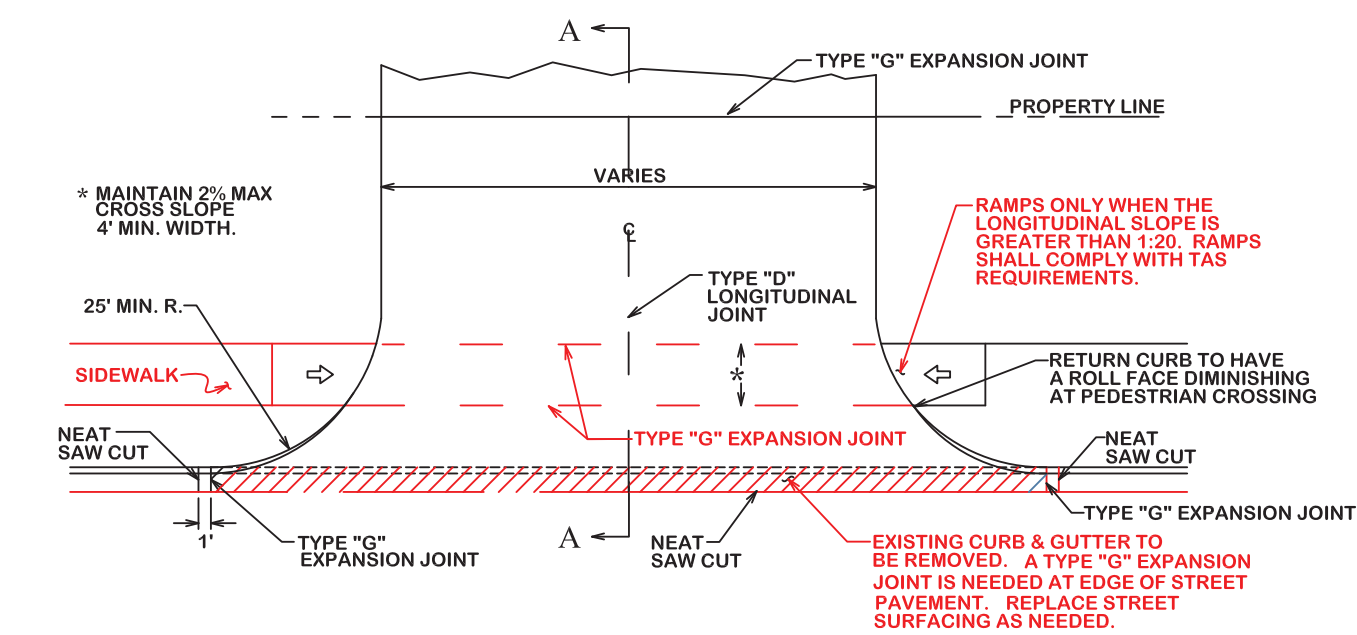
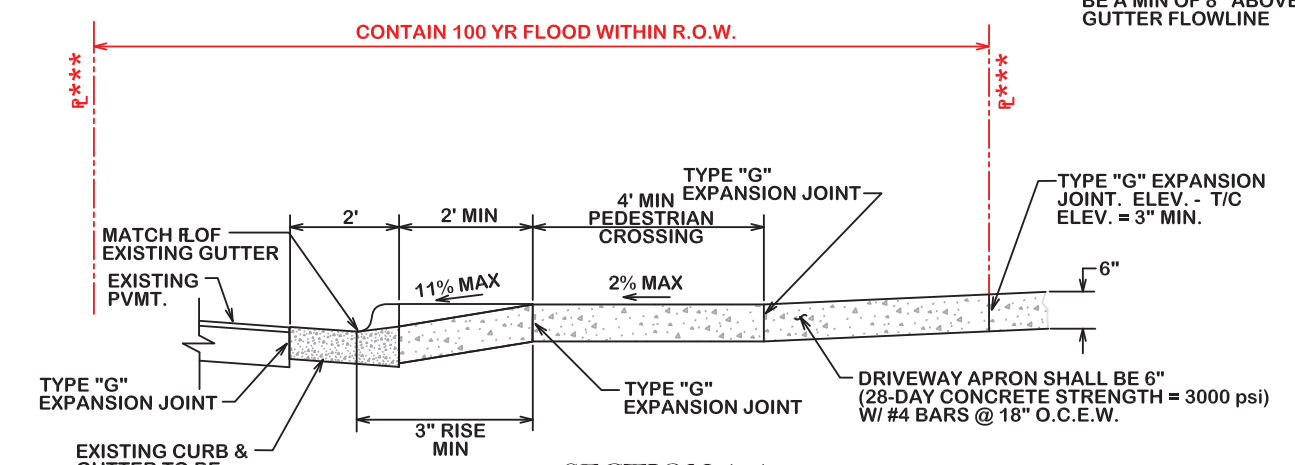


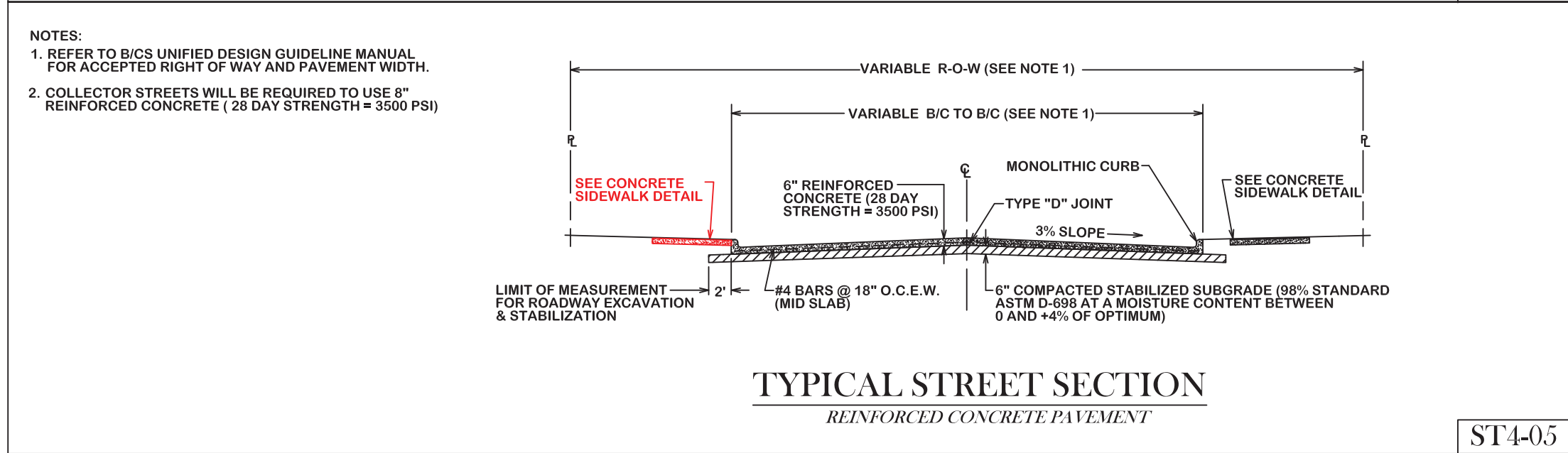
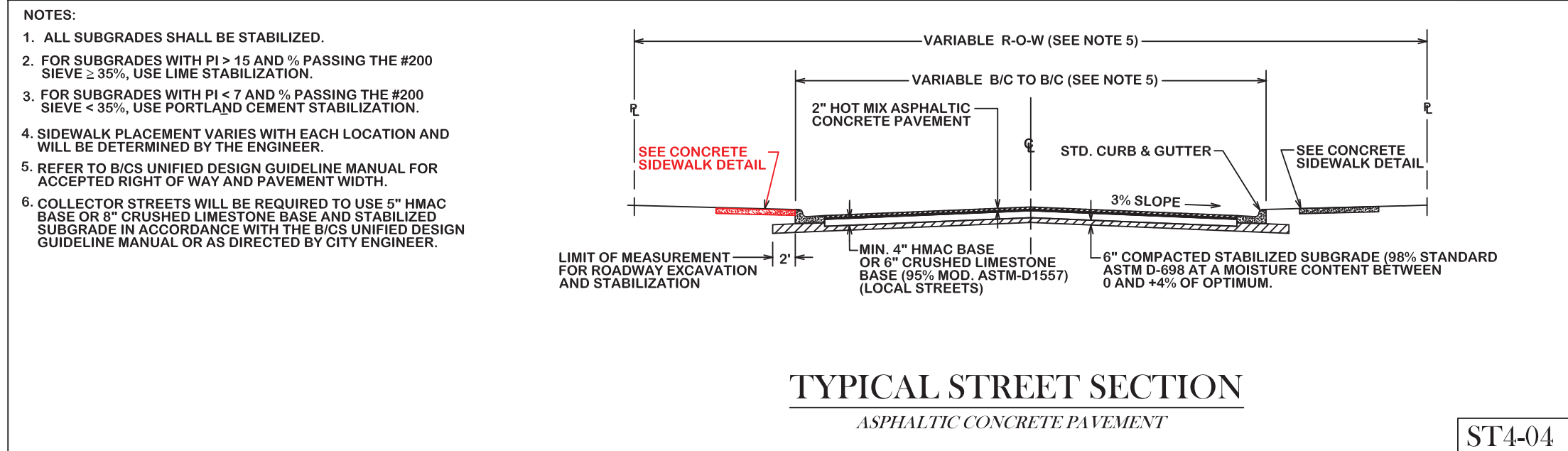
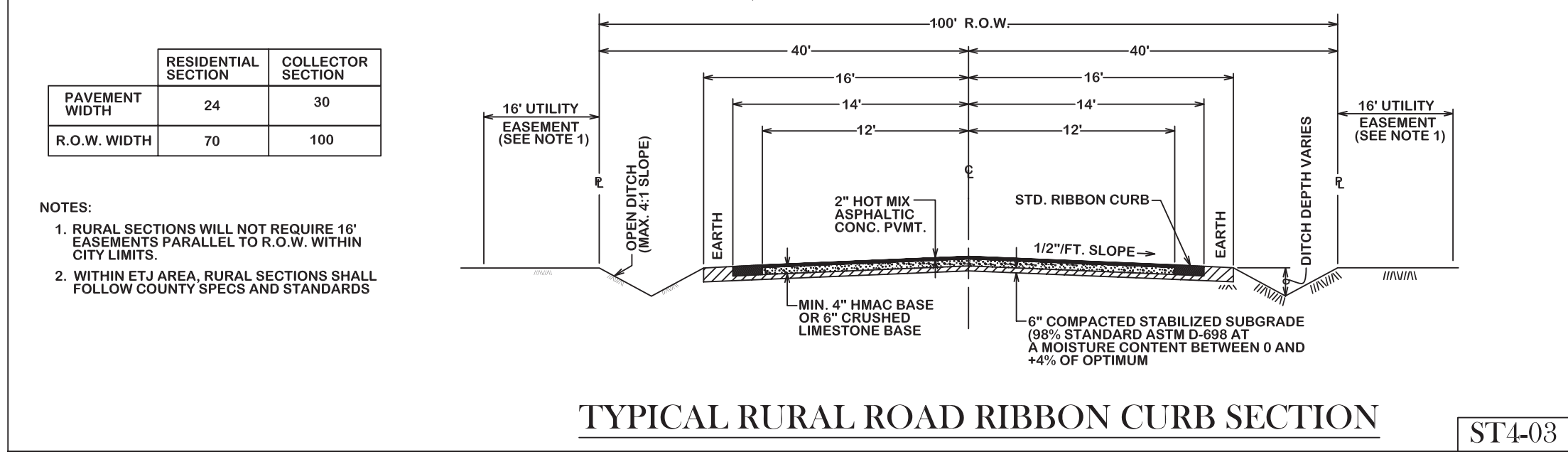
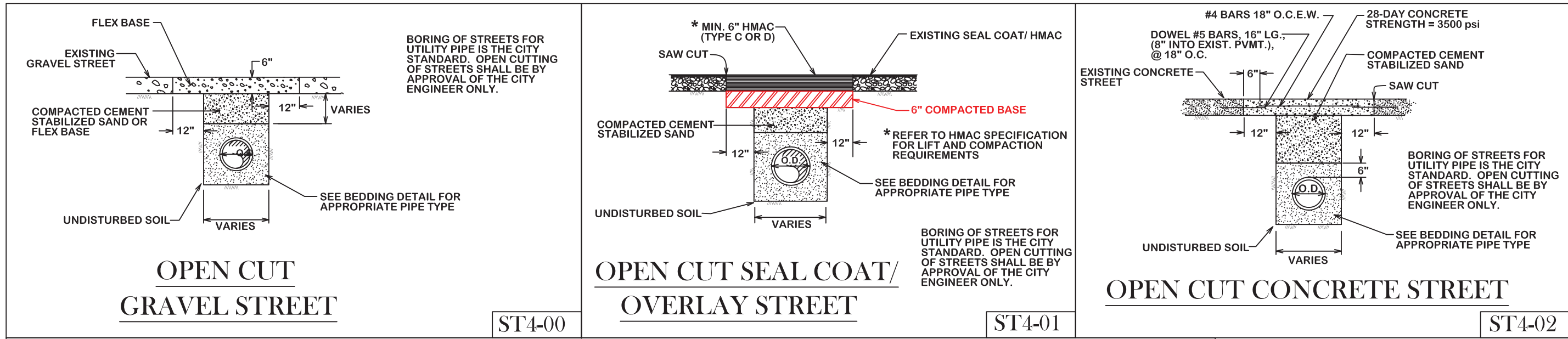
FIGURE:

OT1

STI

CII

SHEET 1 OF 1



REVISIONS:

BRYAN - COLLEGE STATION
STANDARD STREET DETAILS



DRAWN BY: B.I.
DATE: 12/2020
SCALE: N T S
APPROVED: W. P. K.

FIGURE:
ST2
SHEET 2 OF 2